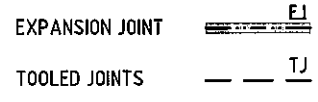


CROSS SECTION A-A

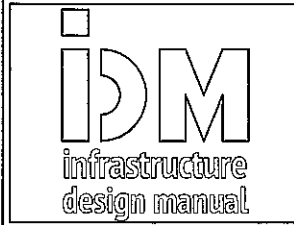
NOTES:

1. CROSS REFERENCES:
INDUSTRIAL CROSSINGS - SD250
RURAL CROSSINGS - SD255 / SD260
IDM - SECTION 12.9.1.
2. THIS DRAWING DETAILS DIMENSIONS FOR STANDARD RESIDENTIAL CROSSINGS ONLY.
3. CROSSING WIDTHS EXCEEDING THE MAXIMUM ALLOWABLE WILL REQUIRE APPLICATION FOR SPECIAL CONSIDERATION.
4. JOINTS AND DOWEL BARS ARE REQUIRED ON EITHER SIDE OF THE CROSSING AT THE INTERFACE WITH THE FOOTPATH. PROVISION SHALL BE MADE IN EXISTING CONCRETE SECTIONS BY DRILLING HOLES TO A MINIMUM DEPTH OF 150mm AND INSERTING R12 X 300mm LONG DOWEL BARS. AN APPROVED JOINT FILLER SHALL BE PLACED ON EITHER SIDE OF THE CROSSING AGAINST FOOTPATH SLABS. DOWEL BARS ARE TO HAVE AN APPROVED BOND BREAKER APPLIED TO THE END OF THE BAR INSERTED INTO THE EXISTING CONCRETE FOOTPATH SECTIONS REFER SD220.
5. ADDITIONAL TOOLED JOINT REQUIRED IF DISTANCE FROM BACK OF KERB TO FOOTPATH IS GREATER THAN 3000 AND SHALL BE PLACED AT THE MIDPOINT OF THE DISTANCE.
6. THE MAXIMUM NUMBER OF CROSSINGS, WHERE ANY CROSSING EXCEEDS 3.5 METRES WIDTH, SHALL BE ONE (1) CROSSING WITH THE MAXIMUM WIDTH OF THAT CROSSING TO BE 6.0 METRES. CROSSINGS TO ADJACENT PROPERTIES SHALL BE EITHER FULLY COMBINED, AND OF MAXIMUM WIDTH OF 6.0 METRES, OR ELSE HAVE A MINIMUM SEPARATION OF 9 METRES.
7. FOOTPATHS OF 75mm THICKNESS ARE ACCEPTABLE ONLY WHERE THE LOTS ARE DEVELOPED ALREADY AND THE RISK OF SITE CONSTRUCTION DAMAGE IS NEGLIGIBLE. WHERE GREENFIELD SITES AND FUTURE HOUSING IS STILL TO BE DONE, THEN THE DEPTH OF THE FOOTPATH SHALL BE 125mm THROUGHOUT.

LEGEND:



ALL MEASUREMENTS IN MILLIMETRES



**RETROFIT RESIDENTIAL
VEHICLE CROSSING DETAIL**

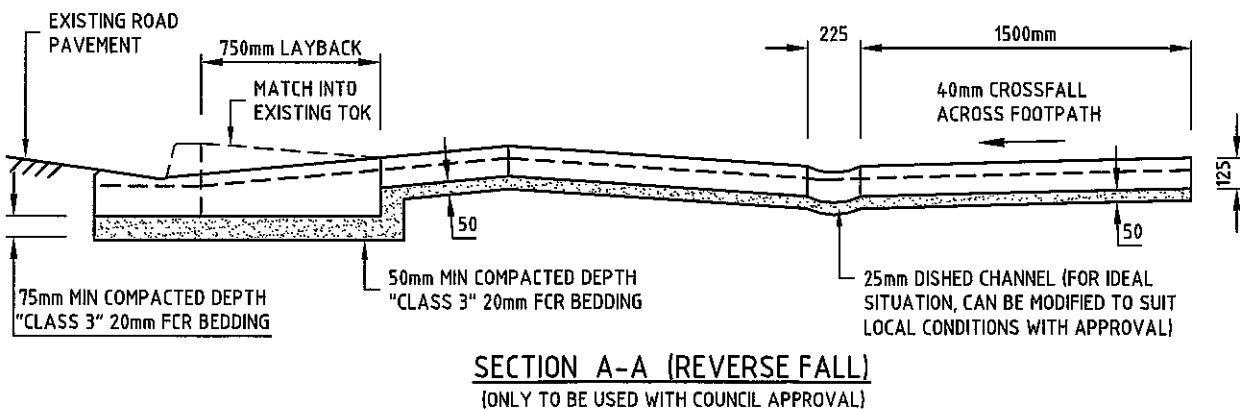
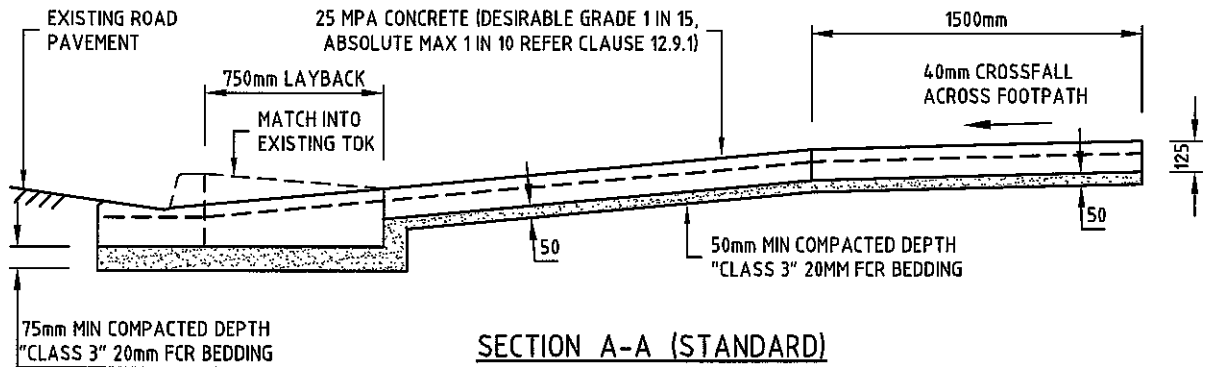
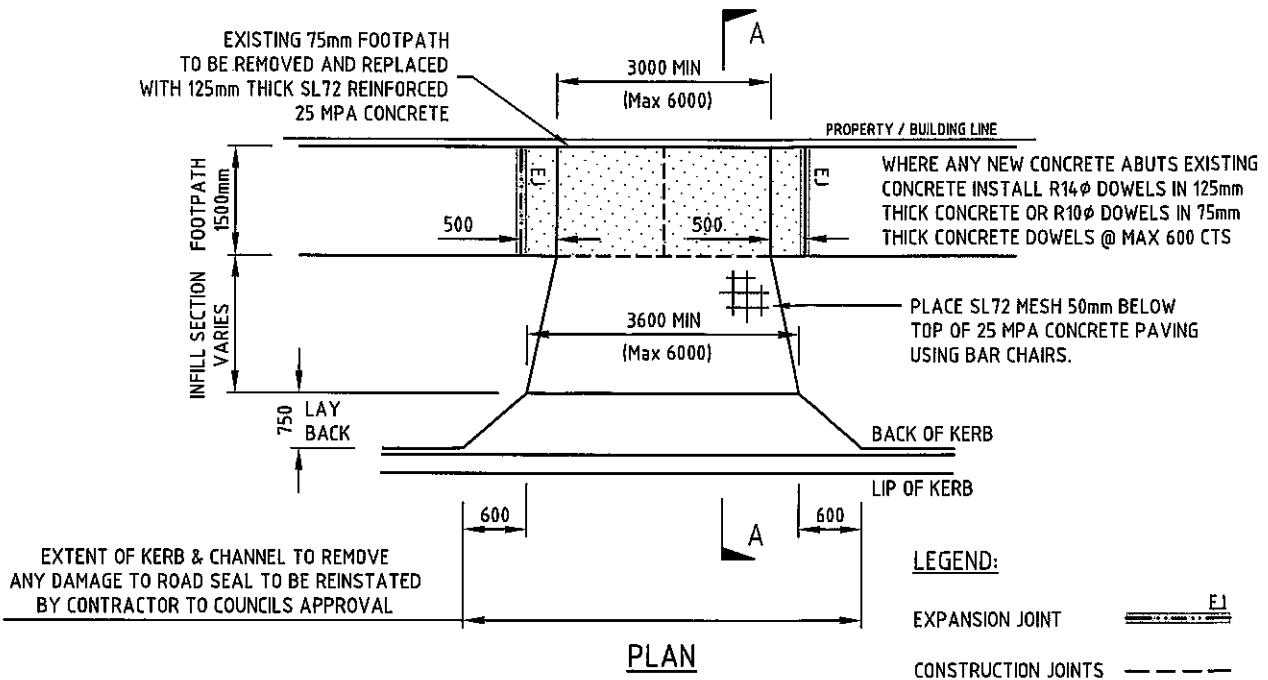
Infrastructure Design Manual Standard Drawings

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www.designmanual.com.au

LAST UPDATED 30/01/2013

SD 235

NOT TO SCALE



NOTE:

FOR GRADES STEEPER THAN 1 IN 15 REFER CLAUSE 12.9.1.
LAYBACK & CROSSOVER, TO BE CONSTRUCTED IN PLAIN CONCRETE ONLY (NO COLOURED CONCRETE BEYOND PROPERTY BOUNDARY)
T.O.K. DENOTES TOP OF KERB

ALL MEASUREMENTS IN MILLIMETRES



NEW RESIDENTIAL SINGLE VEHICLE CROSSING DETAIL

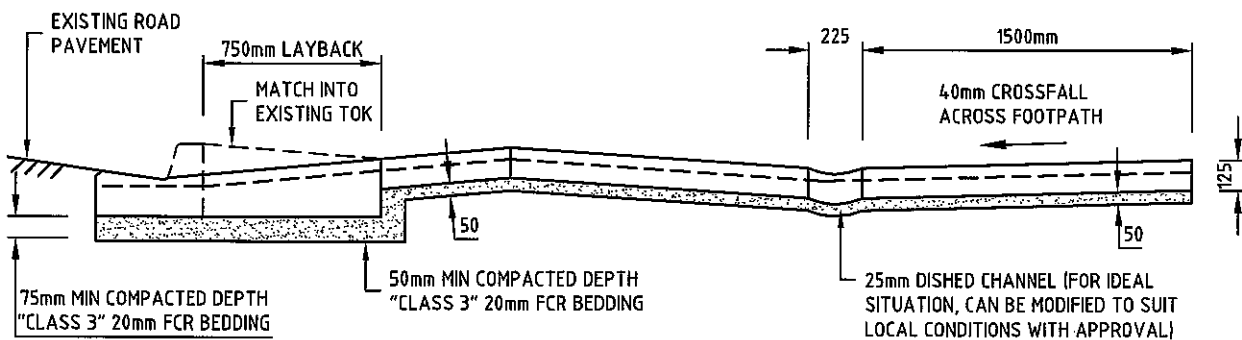
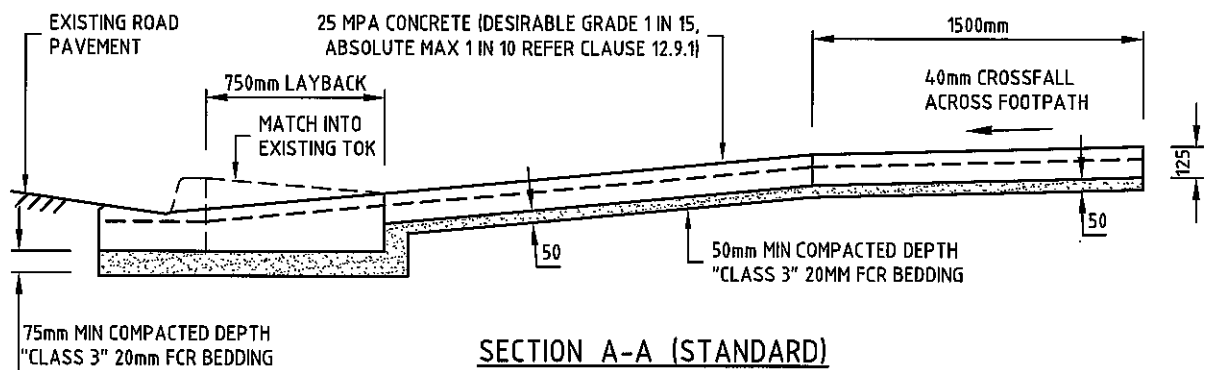
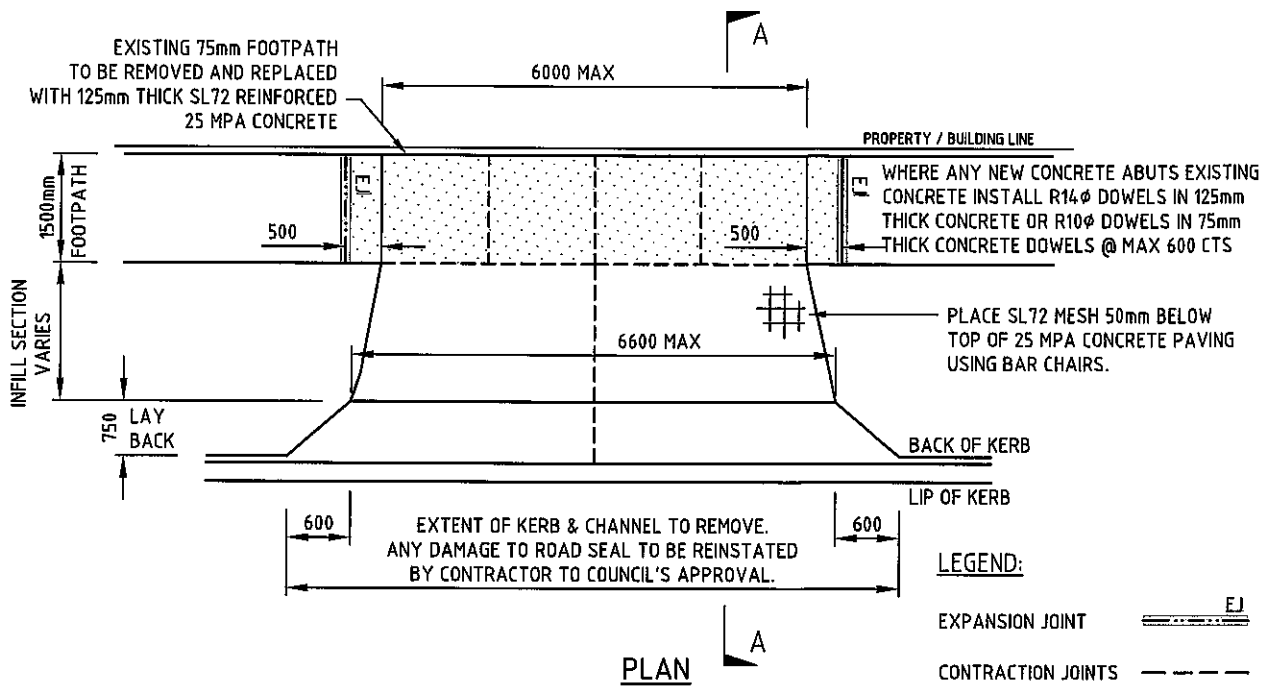
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LAST UPDATED 04/03/2013

SD 240

NOT TO SCALE



NOTE:

FOR GRADES STEEPER THAN 1 IN 15 REFER CLAUSE 12.9.1.
LAYBACK & CROSSOVER, TO BE CONSTRUCTED IN PLAIN CONCRETE ONLY (NO COLOURED CONCRETE BEYOND PROPERTY BOUNDARY)
T.O.K. DENOTES TOP OF KERB

ALL MEASUREMENTS IN MILLIMETRES



NEW RESIDENTIAL DOUBLE VEHICLE CROSSING DETAIL

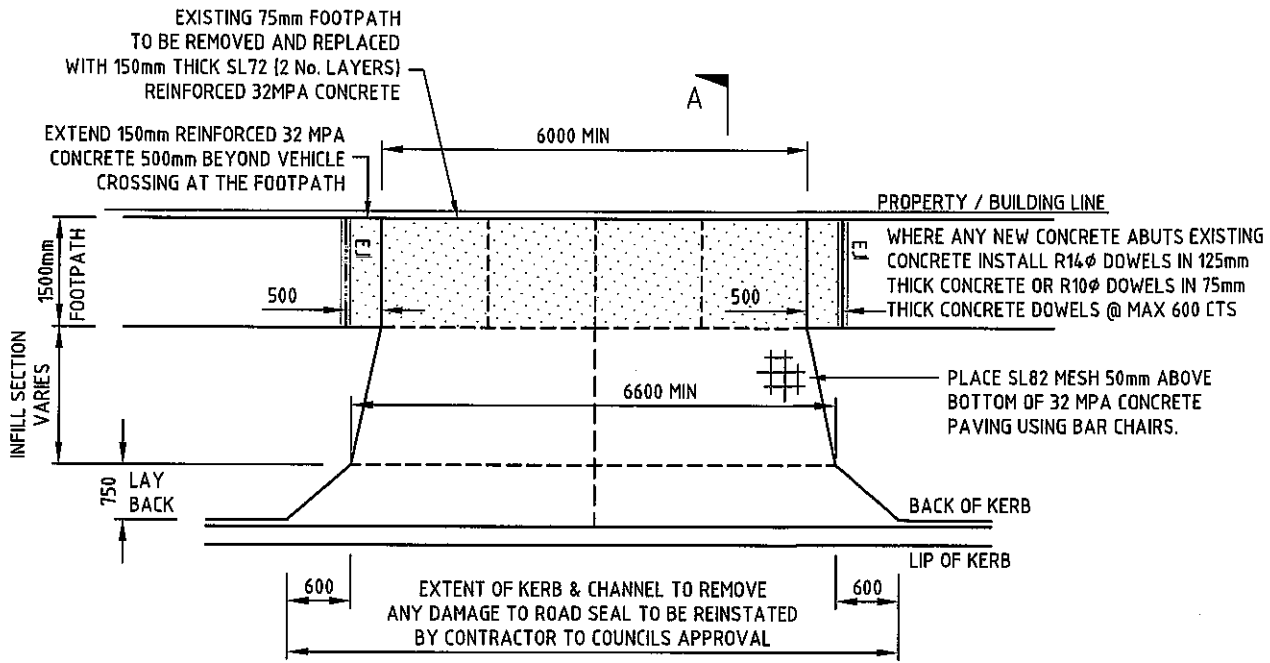
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LAST UPDATED 04/03/2013

SD 245

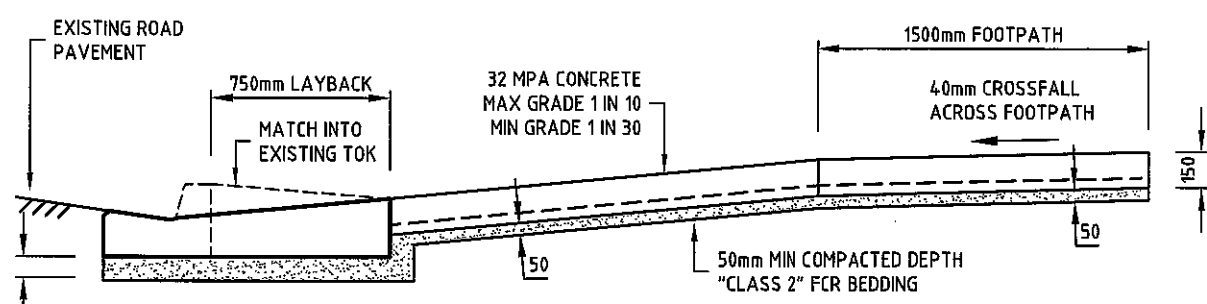
NOT TO SCALE



PLAN

LEGEND:

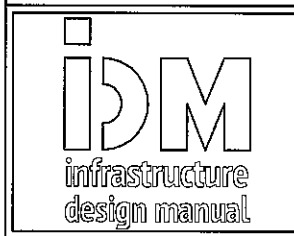
- EXPANSION JOINT
- CONSTRUCTION JOINTS



SECTION A-A

NOTE:
T.O.K. DENOTES TOP OF KERB

ALL MEASUREMENTS IN MILLIMETRES



NEW INDUSTRIAL VEHICLE CROSSING DETAIL

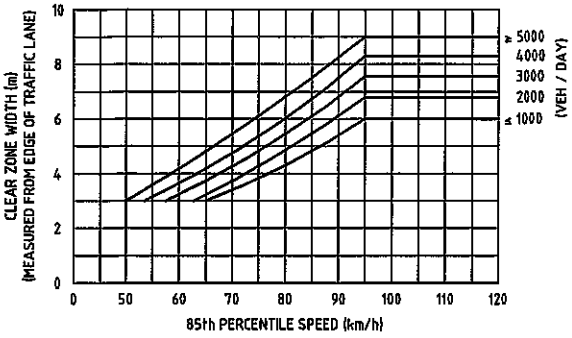
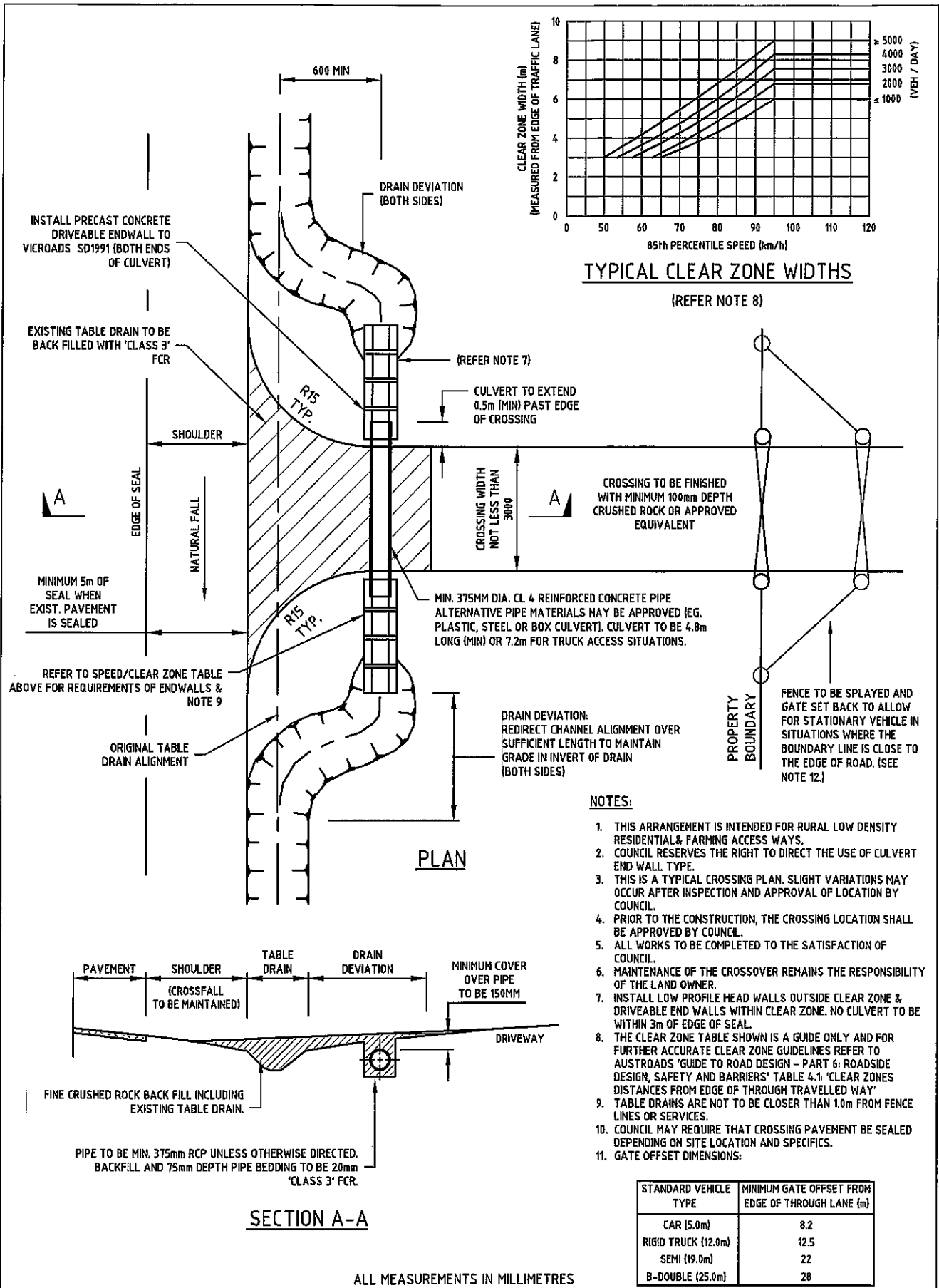
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LAST UPDATED 04/03/2013

SD 250

NOT TO SCALE



TYPICAL CLEAR ZONE WIDTHS
(REFER NOTE 8)

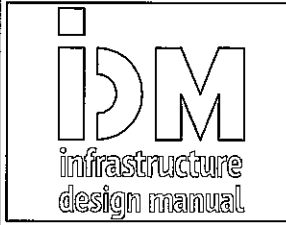
NOTES:

1. THIS ARRANGEMENT IS INTENDED FOR RURAL LOW DENSITY RESIDENTIAL & FARMING ACCESS WAYS.
2. COUNCIL RESERVES THE RIGHT TO DIRECT THE USE OF CULVERT END WALL TYPE.
3. THIS IS A TYPICAL CROSSING PLAN. SLIGHT VARIATIONS MAY OCCUR AFTER INSPECTION AND APPROVAL OF LOCATION BY COUNCIL.
4. PRIOR TO THE CONSTRUCTION, THE CROSSING LOCATION SHALL BE APPROVED BY COUNCIL.
5. ALL WORKS TO BE COMPLETED TO THE SATISFACTION OF COUNCIL.
6. MAINTENANCE OF THE CROSSOVER REMAINS THE RESPONSIBILITY OF THE LAND OWNER.
7. INSTALL LOW PROFILE HEAD WALLS OUTSIDE CLEAR ZONE & DRIVEABLE END WALLS WITHIN CLEAR ZONE. NO CULVERT TO BE WITHIN 3m OF EDGE OF SEAL.
8. THE CLEAR ZONE TABLE SHOWN IS A GUIDE ONLY AND FOR FURTHER ACCURATE CLEAR ZONE GUIDELINES REFER TO AUSTRROADS 'GUIDE TO ROAD DESIGN - PART 6: ROADSIDE DESIGN, SAFETY AND BARRIERS' TABLE 4.1: 'CLEAR ZONES DISTANCES FROM EDGE OF THROUGH TRAVELLED WAY'.
9. TABLE DRAINS ARE NOT TO BE CLOSER THAN 1.0m FROM FENCE LINES OR SERVICES.
10. COUNCIL MAY REQUIRE THAT CROSSING PAVEMENT BE SEALED DEPENDING ON SITE LOCATION AND SPECIFICS.
11. GATE OFFSET DIMENSIONS:

STANDARD VEHICLE TYPE	MINIMUM GATE OFFSET FROM EDGE OF THROUGH LANE (m)
CAR (5.0m)	8.2
RIGID TRUCK (12.0m)	12.5
SEMI (19.0m)	22
B-DOUBLE (25.0m)	28

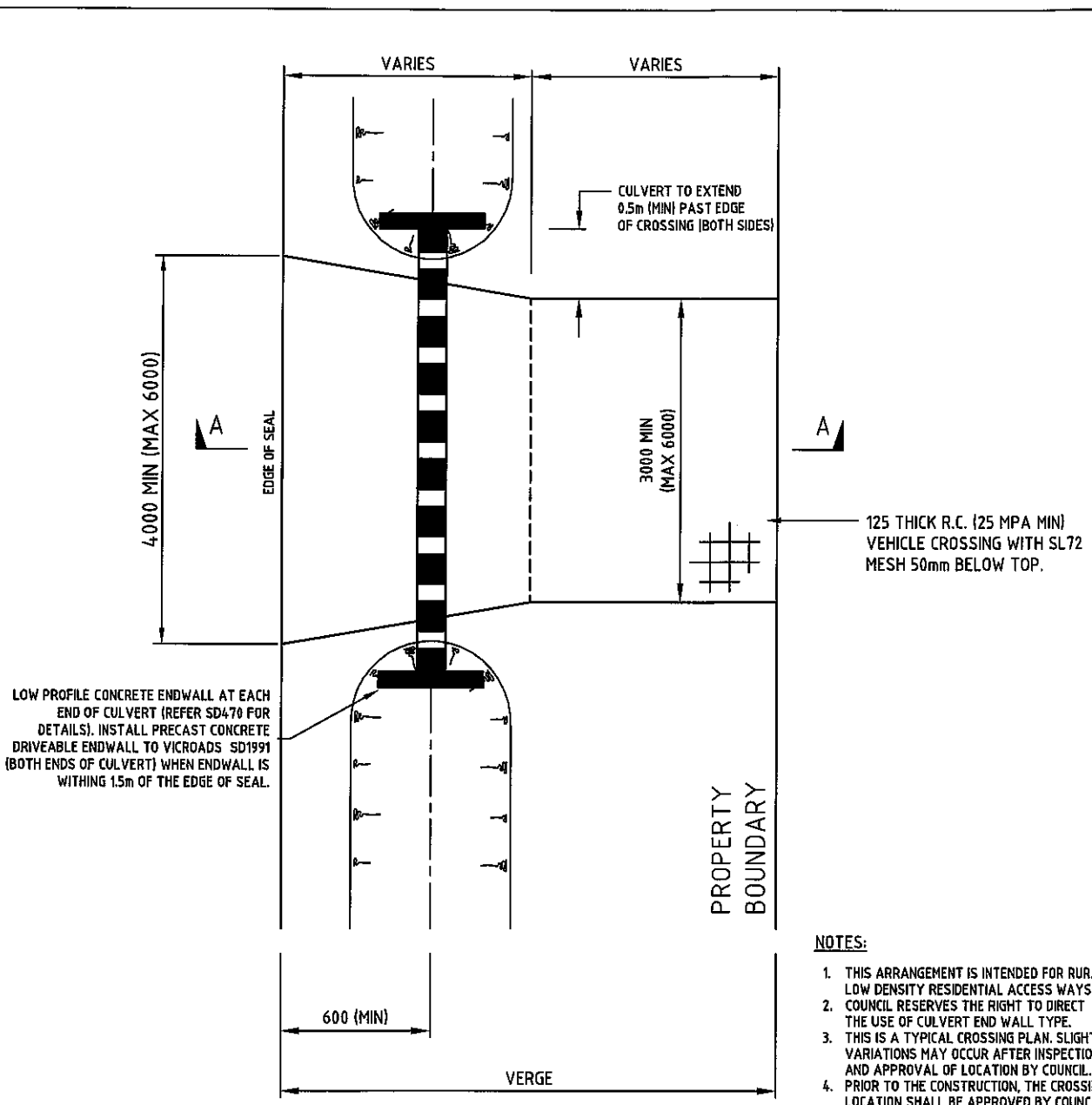
SECTION A-A

ALL MEASUREMENTS IN MILLIMETRES



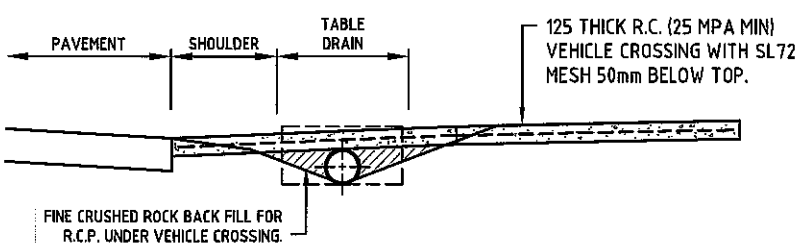
TYPICAL SWALE DRAIN VEHICLE CROSSING (RURAL ENTRANCE)
Infrastructure Design Manual Standard Drawings
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LAST UPDATED 30/01/2013
SD 255
 NOT TO SCALE



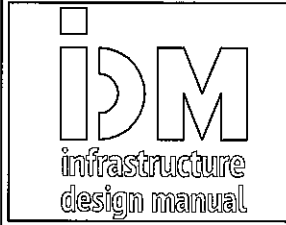
PLAN

- NOTES:**
1. THIS ARRANGEMENT IS INTENDED FOR RURAL LOW DENSITY RESIDENTIAL ACCESS WAYS.
 2. COUNCIL RESERVES THE RIGHT TO DIRECT THE USE OF CULVERT END WALL TYPE.
 3. THIS IS A TYPICAL CROSSING PLAN. SLIGHT VARIATIONS MAY OCCUR AFTER INSPECTION AND APPROVAL OF LOCATION BY COUNCIL.
 4. PRIOR TO THE CONSTRUCTION, THE CROSSING LOCATION SHALL BE APPROVED BY COUNCIL.
 5. ALL WORKS TO BE COMPLETED TO THE SATISFACTION OF COUNCIL.
 6. MAINTENANCE OF THE CROSSOVER REMAINS THE RESPONSIBILITY OF THE LAND OWNER.
 7. DRIVEABLE ENDWALLS TO BE USED WITHIN 1.5m OF THE EDGE OF SEAL
 8. TABLE DRAINS ARE NOT TO BE CLOSER THAN 1.0m FROM FENCE LINES OR SERVICES.



SECTION A-A

ALL MEASUREMENTS IN MILLIMETRES



TYPICAL SWALE DRAIN VEHICLE CROSSING (FRINGE URBAN RESIDENTIAL ENTRANCE)

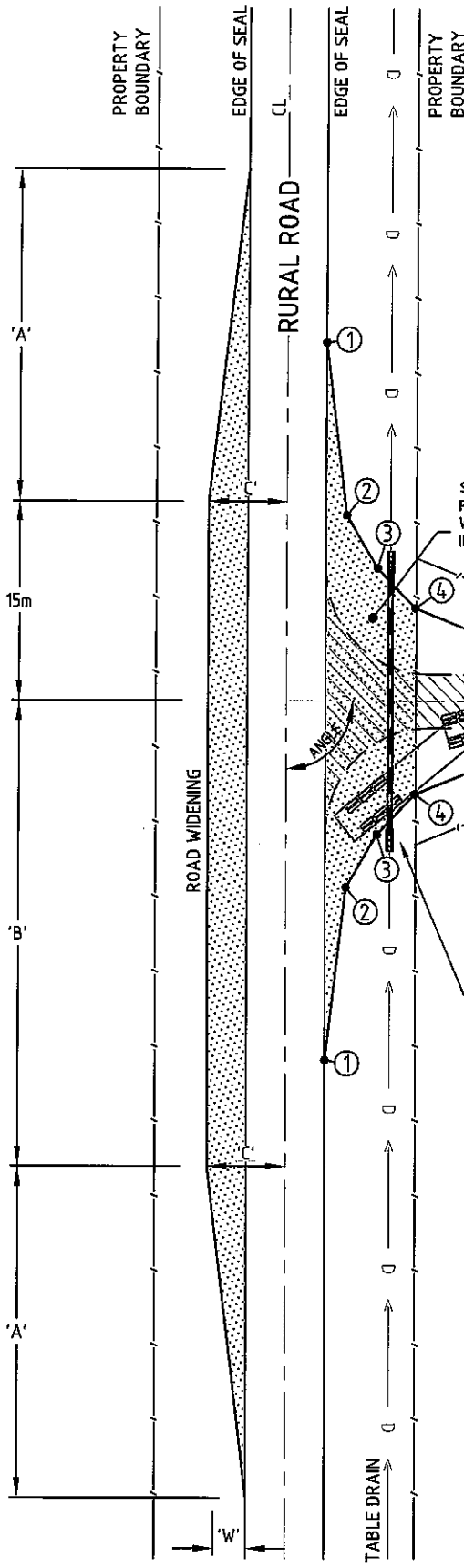
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LAST UPDATED 30/11/2011

SD 260

NOT TO SCALE



SETOUT TABLE:

POINT No.	OFFSET DISTANCE FROM EDGE OF SEAL (m)	OFFSET DISTANCE FROM ACCESS CENTRELINE (m)
1	0.0	27.0
2	1.6	14.0
3	4.0	10.0
4	6.9	7.0
5	18.3	2.5
6	22.0	2.0

ROAD WIDENING:

A = $\frac{0.5 \times \text{SPEED (km/h)} \times W}{3.6}$

ANGLE:	70°	90°	110°
LENGTH (m):	40	35	30

C = ON STRAIGHT: 6.0m (MIN)
ON CURVE: 2 x [3.0m + CORRESPONDING WIDENING FOR CURVE RADIUS]

W = FORMATION WIDENING (IF REQUIRED BY COUNCIL)

LEGEND:

TYPICAL EXISTING ACCESS =

RECOMMENDED ACCESS SPLAY =

AREA TO BE SEALED =

- NOTES:**
- THIS ARRANGEMENT IS INTENDED FOR RURAL / FARMING ACCESS WAYS THAT REQUIRE SEMI / B DOUBLE ACCESS.
 - PAVED AREAS TO BE A MINIMUM OF 150mm DEPTH COMPACTED GRAVEL. PAVEMENT AREAS & TYPE TO BE APPROVED BY COUNCIL.
 - COUNCIL RESERVES THE RIGHT TO DIRECT THE USE OF CULVERT END WALL TYPE.
 - THIS IS A TYPICAL CROSSING PLAN. SLIGHT VARIATIONS MAY OCCUR AFTER INSPECTION AND APPROVAL OF LOCATION BY COUNCIL.
 - PRIOR TO THE CONSTRUCTION, THE CROSSING LOCATION SHALL BE APPROVED BY COUNCIL.
 - ALL WORKS TO BE COMPLETED TO THE SATISFACTION OF COUNCIL.
 - MAINTENANCE OF THE CROSSOVER REMAINS THE RESPONSIBILITY OF THE LAND OWNER.
 - REFER SD 255 FOR CLEAR ZONE OFFSETS.
 - DRIVEABLE ENDWALLS TO BE USED INSIDE CLEARZONE.
 - TABLE DRAINS ARE NOT TO BE CLOSER THAN 1.0m FROM FENCE LINES OR SERVICES.
 - COUNCIL MAY REQUIRE THAT CROSSING PAVEMENT BE SEALED DEPENDING ON SITE LOCATION AND SPECIFICS.
 - GATE OFFSET DIMENSIONS:

STANDARD VEHICLE TYPE	MINIMUM GATE OFFSET FROM EDGE OF THROUGH LANE (m)
SEMI (19.0m)	22
B-DOUBLE (25.0m)	28

ALL MEASUREMENTS IN MILLIMETRES



TYPICAL B DOUBLE VEHICLE CROSSING (RURAL ENTRANCE)

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LAST UPDATED 30/01/2013

SD 265

NOT TO SCALE